

THE BRASS POUNDER



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Kansas Central
Division NMRA



Mid-Continent
Region NMRA



National Model
Railroad Association



Kansas Central Division Call Board

Superintendent

Ross Boelling
rkboelling@gmail.com

Clerk

Tom Katafiasz

Paymaster

Larry Tiffany

Brass Pounder Editor

Ray Brady

BP Associate Editor

Christine Heinsohn

Director

Ray Brady
joycove@wilsoncom.us

Join the KCD on Facebook

Kansas Central Division-NMRA has a Facebook page and is for NMRA members only. It is a place to share model railroading adventures, post pictures, tell tales of woe in building your empire, post obstacles you have overcome, and ask questions. Have Fun. Simply enter "KCD" in the Facebook search block and select "request to join". See you there.

Looking Ahead

KCD Zoom Business Meeting

Saturday, December 4, 2021 @ 7:00PM @ Larry Tiffany's

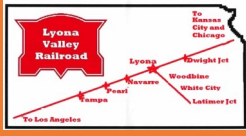
KCD Elections for Superintendent and Director

Ballot emailed out by December 18, 2021

Voting deadline to return ballot to Larry Tiffany—January 10, 2022

KCD ZOOM Business Meeting

Saturday, February 5, 2022



Superintendent's News

By Ross Boelling

I am having a hard time focusing on all my jobs. The phrase "jack of all trades, master of none" comes to mind these days.

We have spent the past two months shifting our residence from the barndominium to the farmhouse across the driveway. We lived in the house from '86 to '91, then Lynnette's brother's family lived here for 10 years, and finally Lynnette's folks lived here until last May when her Mom moved to Abilene. We decided in late July that we would move to the house.

So..... That meant clearing out her mom's stuff and moving ours in. We have taken about a ton of stuff to the dump and about that much to a local second-hand store. We ended up having to refresh all the house, I installed a new breaker box and did a fair amount of rewiring, I updated the plumbing and we had air conditioning and a new furnace installed, entirely painted and updated the first floor, new carpet, etc. Well you get the jist of what was involved.

I have also volunteered several times as Conductor for the ASVRR and I am starting to learn how to drive ASVR #4. Being President of the Kansas Silver Haired Legislature, Chairman of the Lions Club Zone, and President of the Woodbine Lion's Club means I have little time left to do anything real or model railroad related.

I have reached a point where I wonder if I have the stamina and interest in taking on Herington Railroad Days or continuing as a KCD officer. The way my schedule is shaping up, it

looks like it may well be January before I am able to dedicate any appreciable time again to the Lyona Valley Railroad. Sigh! I do not know what to do. And then throw in the three granddaughters' school and sports events...

Maybe you have been in this place before. Overwhelmed. But then the new Model Railroader, Cowcatcher, or NMRA journal shows up and I see the photos and articles of what everyone has accomplished, and I feel a tug of motivation. Plus, when I participate in our ZOOM meetings and Turntables and see what everyone has been working on, I feel the pull towards action. So, I guess there is hope.

I often dream of completing the LVRR, even though now I will have to walk to it from the house instead of just having it downstairs in the barndominium. I can visualize the operating session with 5 or 10 folks operating it like a well-oiled symphony. But currently, my reality and my virtual reality are really at odds with each other. And I am aware of the strategy of just working 10-20 minutes a day on it and soon you have something done. But I am not there.... Yet...

Thanks to everyone for their continuing support of KCD and the hobby.

Do not forget we have nominations at our December meeting for Superintendent and Director for three-year terms!!

Happy Thanksgiving and Merry Christmas!!

*Ross Boelling, Superintendent
Kansas Central Division*

Kansas Central Division 2021 Elections

From the 2020 KDC Constitution and Bylaws

Summary of the Election Process:

- Step 1:** Nominations submitted by end of the December Business meeting, or Dec 1 if no December meeting is held.
- Step 2:** Paymaster submits (email) ballot to members no later than third Saturday of December.
- Step 3:** Members return ballot to Paymaster no later than second Monday in January.
- Step 4:** Paymaster report results at the Annual Election Meeting. (First Business meeting of the year)

Office	Ballot years - to take office the following year			
Superintendent	2021	2024	2027	2030
Director	2021	2024	2027	2030
Clerk	2020	2023	2026	2029
Paymaster	2019	2022	2025	2028

Duties of the Superintendent and Director are as follows((from the KCD Bylaws found here)

<https://www.dropbox.com/s/8bdnw2ctmjssim7/Final%20KCD%20Constitution%20and%20Bylaws%202020.pdf?dl=0>

Section One:

The duties of the Superintendent shall be to:

- a. *Provide vision and leadership for the Division.*
- b. *Plan Division meetings and content in conjunction with the Division Officers.*
- c. *Encourage member's participation in Division activities.*
- d. *Preside at all Division meetings.*
- e. *Represent the Division at all Region meetings and functions at the request of, or in the absence of, the Division Director.*
- f. *Assume the office of Director for the interim if it is vacated, serving until a successor can be elected as provided in the Constitution.*
- g. *Approve all Division disbursements and be an alternate signatory on the Division Bank account(s).*
- h. *Provide for an annual audit of the Division bank accounts.*
- i. *Appoint a Division Editor/Publisher whose duties shall be to publish:*
 - . *Minutes of all business meetings.*
 - . *Reports from the Superintendent and Director.*
 - . *Division activities.*
 - . *Member-submitted modeling and other railroad-related activities.*
 - . *Newsworthy information from the Division, Region, and National.*
 - . *Historical information of interest from the Division, Region, or National.*

Section Two:

The duties of the Division Director shall be to:

- a. *Represent the Division at the Region.*
- b. *Preside for the Superintendent in his absence.*
- c. *Plan meetings and content in conjunction with the other Division Officers.*
- d. *Assume the office of Superintendent for the interim if it is vacated, serving until a successor can be elected as provided in the Constitution.*
- e. *Provide minutes of all business meetings in the absence of the Clerk.*



Director's News—The Tulsa Convention

By Ray Brady

By my estimation, the 2021 MCoR convention was a success. It was a joint effort of our own Mid-Continent Region and the Lone Star Region, and it fulfilled my expectations. It was a tremendous learning experience throughout.

From the conventions I have attended, there are six basic offerings at a convention: **Clinics, Layout Tours, Operating Sessions, Contest Room, Socialization, and Area Attractions.** The Tulsa Convention offered 21 different clinics and 14 Layouts, and, at the behest of the many Tulsa area modelers that were offering either lay tours or operating sessions, the clinics were given blocks of time different from operating sessions or layout tours so the layout people could attend the clinics. And the Operating sessions were at different times than the Layout tours - same reason. And the Contest room was open all the time - up until the evaluations began. So, the Tulsa convention offered the best of all worlds for everyone.....

Clinics: My primary “go to” events at conventions has been to attend clinics. These are a great way to pick the brains of modelers, get ideas for my modeling, and carry them back home.

In all, I only attended 4 of the clinics—with double that number of opportunities available. I normally concentrate on clinics, so this was less than normal, but more than adequate to satisfy me because it allowed me to branch out to the other areas mentioned above. The clinics I attended were:

- a. Ken Ehlers’ clinic (from the Indian Nations Division MCoR) as he described his **Pandora and San Miguel Railway** layout design compromises he made as he attempted to loosely follow the **Rio Grande Southern** prototype—including operations.
- b. Dean Smith’s clinic (from the Central Missouri Area MCoR) as he described his **East Tennessee & Western North Carolina Railroad** and how he incorporated Operations. The most important aspect of his clinic to me was that FUN was an integral part of his layout objective,

with may anecdotes given as we “virtually” toured his layout via PowerPoint. Hopefully you’ll read more about this in the MCoR Caboose Kibitzer....

- c. Duane Richardson (Lone Star Region) and Whit Johnson’s (MCoR) clinic about modeling for the **Achievement Program** and/vs a **Convention Contest Room.** After a brief introduction to get the discussion going, this clinic evolved into a series of Q & A about these separate but related subjects.

Because of my recent receipt of four AP awards by virtue of just “doing my model railroad,” I had an interest in learning more about “do’s and do not’s” for other AP awards that would involve building motive power, cars, or structures (as those were the areas I would have to tackle.)

Even though I have been writing about others getting these awards, it still was a little intimidating to think about starting the model building aspect of the AP program. Look for an article in the next Caboose Kibitzer as I condense my tape recording of the clinic to the written word.

The AP program is not that mysterious, and it is intended for you to become a better modeler. The program is not about being a test - it is about offering continuous improvement in your modeling activities.

- d. Finally, Larry Alfred, MMR’s clinic (Turkey Creek Division MCoR) gave a clinic on **Building with Cardstock.** Because of my recent foray into cardstock buildings (you have read about in the April 2021 Brass Pounder), I wanted to hear more about what Larry had to say. And, the part of my backshop building that had me in paralysis suddenly got clarity and I think I can now proceed with more of my UP Cheyenne Backshop. YESSS!!!!

I had wanted to attend other clinics, but the opportunity for other endeavors reared its head...

Layout Tours: I took advantage of the allocated

times in the schedule to drive to two layouts. The Indian Nations Division members are a hotbed of modelers and they have produced some fabulous layouts. There were 12 layouts of the 14 listed available for tours (2 provided operations, but no tours).

I had already operated on one - David Steensland's HO_{n3} **Silverton and Lake City Railroad** a couple of years ago. So, I chose to go see Jim Sinclair's HO **Southern Pacific over Donner Pass**, and Ken Ehlers' Sn3 **Pandora and San Miguel** modeled after the Rio Grande Southern in September 1942. Both were fabulous layouts.

Jim's SP layout fills a separate building that I would estimate to be 30' X 48'. It is HUGE, complete with elevation changes representative of the prototype.

Ken's layout was in an upstairs room in his house that was 23' X 23.5' and was simply fabulous. The scenery was superb, with the Rocky Mountain forest very realistic, and the layout was complete with photo backdrops of the prototype area being modeled. Diagrams and some pictures of these layouts are shown on the following pages.

Operating Sessions: I did not attend any operating sessions at this convention. But, as I indicated earlier, I previously had operated on Dave Steensland's **Silverton and Lake City Railroad**. But, I can tell you that Indian Nations Division is a hotbed of operations layouts.

As indicated above, there were 14 layouts available for operations at multiple times from Wednesday through Sunday. The layouts are sized to have from 2 to 16 operators. It is clear that the Tulsa area modelers take prototype railroad operations seriously. If you are in to operations, there were multiple opportunities to sign up for operating sessions on very diverse types of railroads. There is something for everyone.

Contest Room: The contest room offers each of us an opportunity to show off what we can do, as well as offer up to our peers the chance for them to suggest to us the possibility for continuously improving our modeling.

Part of the purpose of the contest room is to have the models evaluated for the AP program,

and part of the purpose is to allow the convention attendees make a group evaluation of "best of show," etc.

At the employer I retired from, the culture that was constantly being instilled in us was the concept of "Continuous Improvement." Continuous improvement gives us the opportunity to evaluate where we are, decide what we can do better, and go forward with a better product. It is a learning process.

That is the concept employed with the Contest Room. The intent is not to get a "grade" about how well we did with a particular model. But rather the intent is to give us the opportunity to improve our skills by getting feedback.

On the evening when the evaluations were to begin, I happened to be just coming out of the Contest Room when I was corralled to participate in the evaluations. After much denial on my part about being qualified, I was finally roped-in to participating. After an indoctrination, I was teamed up with two other experienced individuals and off we went.

There were over 100 entries in the various categories. We were given a specific evaluation category (*Construction, Detail, Conformity, Finish & Lettering, or Scratch Built*) to evaluate for each of the models. It took us over 5 hours to go through the models. It was tedious, but it was also a learning experience. I appreciated that.

Each of the above areas have NMRA guidelines for what the evaluators are looking for as well as a matrix of how to score the models (<https://www.nmra.org/forms>). In addition, the Lone Star Region has supplemental information on convention contest room evaluations (<https://www.lonestarregion.com/node/6>). I encourage you to read through the information in the indicated links to understand better the opportunity that the contest room has for you to improve your modeling skills.

Social Interaction: Of the conventions I have attended, there always has been a location available for conventioners to just "sit and talk." There is a tremendous camaraderie built up over the years by many Model railroaders and they just enjoy getting together and "spittin' and whittlin'" or "knittin' and pearlin'" (yes, there were women

at the convention as either modelers or spouses).

This Tulsa convention was no different. The hotel had an atrium and there were groups gathered there throughout the convention. I joined groups at various times, and was always welcomed to the group.

While I am a relative newcomer to the NMRA, it was obvious from the chatter that some participants go back as much as 40-50 years. It was fun to listen and participate in the conversation. And, this convention was even more special because the Texas and Louisiana folks from the Lone Star Region joined the conversation.

Tulsa Area Awareness: All was not just about planned events at the convention site. In the course of driving to the Layout Tours, I also had done some “Googling” and found some other model railroad points of interest.

One was a Model Railroad store that specialized in N-scale. For me, that was a must-see. I didn’t spend much money there, but it gave me the opportunity to see an n-Trak reversing module that was under construction by the n-Trak group that called the store home. It was of interest because the Topeka n-Track club to which I belong has been entertaining

alternate module arrangements other than a “circle” arrangement. So, that part of trip was equally educational.

And of course there was a Model Train Show just south of Tulsa that “just happened” to be along the route to one of the layouts on the layout tour. So, of course I had to go in to see what people had.....

Summary: All-in-all, the Tulsa Convention offered a plethora of things that any model railroader would find informative. We are all different. We all have a different vision of how we want to do “model railroad.” It is important that we recognize that. And, the very nature of the Tulsa Convention format strengthened the opportunity for all of us to grow where our interests take us. You missed a huge opportunity if you didn’t go to the convention.

Don’t forget that August 7-13, 2022 offers an even bigger opportunity in St. Louis at the National NMRA convention. You’ll be sorry if you don’t go.....

One final note: The conventions normally close out on Saturday evening with a banquet and the presentation of awards from the Congest room, as well as various awards from the Mid-Continent Region. This conference was different only in that the hotel kitchen was being remodeled so the “Dinner” became a “Catered Dessert.”

One of the MCoR awards given out is the “*Larry R. Long MMR Award*” given in recognition of dedicated service as the Mid-Continent region’s Division volunteer of the Year. If you remember, our own David Heinsohn received the award in 2017.

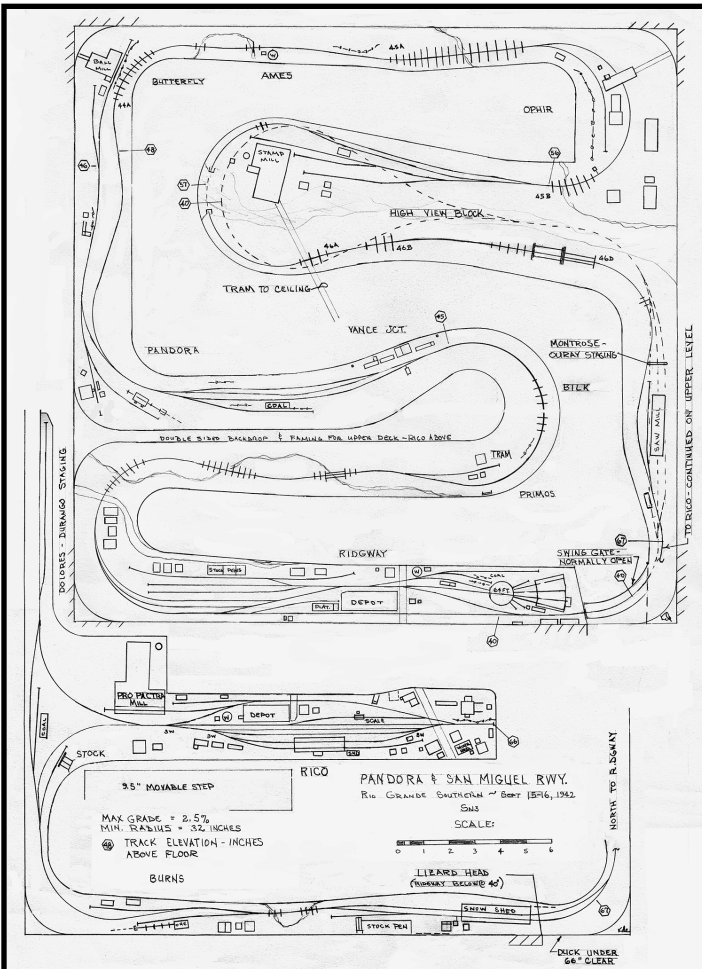
Well, this year another Kansas Central Division member received the award. The picture below is me receiving the 2021 award from the 2020 award recipient, Larry Alfred. This was totally unexpected.... Thank you to the previous three recipients (Larry Alfred, MMR, Miles Hale, and David Heinsohn) for selecting me for the award. (and yes, there is a story behind the double shirt....)

Ray





Ken Ehler's Layout



Rock Island Alco Century 415's in HO Scale

Part One

By Greg Schneider

The advent of 3D printing has done wonders for the model railroad hobby, particularly when it comes to rare locomotives which have not been available in any shape or form (or were available but were not suitable for the discriminating hobbyist).

One of these locomotives was Alco's Century-415—the diminutive, 1500 horsepower center-cab switcher which was produced from 1966-1968. There were not many takers for the C-415, with only 26 units in all working for seven owners, including the Rock Island, who purchased five of the units on its Union Pacific lease account. The Rock, known for its love of oddball locomotives, bought ten of them, painting some in the maroon and yellow colors of the 1960s and some in the bright red and yellow Material Service colors which dominated Rock Island units until John Ingram's infamous blue and white "The Rock" scheme emerged post-1975. Only one ex-Rock Island Century survives today, number 423, on the Buffalo Southern at Hamburg, New York.

The Alco C-415's on the Rock Island prowled the southeast side of Chicago, working transfers between Blue Island and South Chicago yards, through Pullman Junction (the area I model in HO scale). In typical Rock Island fashion, it would be possible to see the 415's with odd assortments of other locomotives, including a rebuilt RS3m, with EMD engine and GP-7 body. Befitting the Rock Island's financial problems, the units were filthy and were operational headaches. All but one made it until shutdown in 1980 and two plied the rails in Clay Center, Kansas for the Brandon Railway. The remainder were scrapped. (The October 2021 issue of *Railfan and Railroad* has an excellent article on the C-415's).

Years ago, International Hobby Corporation developed an HO model of the Century 415. It was toylike, with terrible power assemblies and unworkable from a kit bashing perspective, though it has been accomplished by many brave

modelers. There are also brass models floating around, but hard to find and even more expensive to acquire. Many hobbyists thought Bowser would take the plunge on the C-415 but given the few units sold to railroads, it is not surprising that a mass manufactured HO scale C-415 has never emerged.

What to do? These units were crucial to 1970s southeast side railroading in Chicago. Thank goodness for eBay. While searching one day, I came across a 3D printed Rock Island C-415 issued from Pacific Northwest Resins located in San Juan, Texas (of course). I quickly bought three. They recommend an RS-3 powerbase, so I purchased three RS-3s on Ebay as well. Eureka! Problem solved.

The shells were each molded to specifications of the individual railroad who had different trucks and details for its C-415s. They needed some additional pruning and cleaning up and were lacking sufficient details. It quickly became a detail project. Again, we live in a good era to find the right detail parts, whether it is old Detail Associate air horns, bells, horns, or Cal Scale models parts. The Rock's units had a bell mounted front center, which I found on Cal Scale models. They used the Nathan airhorn, which I found from Detail Associates. Alco detail parts from Cal Scale also helped with handrails. I even found the unique exhaust which the Rock Island units had right outside the window of the center-cab switcher (talk about breathing problems for the crew). It curved like a letter S and will fit snugly between the windows on the locomotive. That part will be painted blackish silver.

Adding handrails and stanchions is always a bit daunting. I wanted to reuse the plastic handrails from the RS-3 units I had acquired, but they wouldn't stay put. I bought stanchions from Cal Scale and attached those in a hole I drilled and with CA cement. Then I ran brass wire through the stanchions, shaped them to fit and I had my

handrails—they are not perfect, befitting the Rock Island, and it may be worthwhile to redo them. Many of the handrails will have to be redone which I will also tackle at the end of the project.

I primed the locomotives with Testor's white primer (**Figure 1**). I chose Tru-Color Rock Island maroon and Rock Island yellow. I have painted all three in this scheme (**Figures 2 and 3**).

My next step is to apply decals, and weather (heavily) the units. The first step will be to fade the dark glossy paint, weather them like they were beaten and used in harsh conditions (and not cared for), and assemble the other parts (fuel tanks, trucks etc). Then I will see if the RS-3 assembly really works for the unit, add digital

decoders with sound and get them on the layout hauling freight. More to come in the next issue.

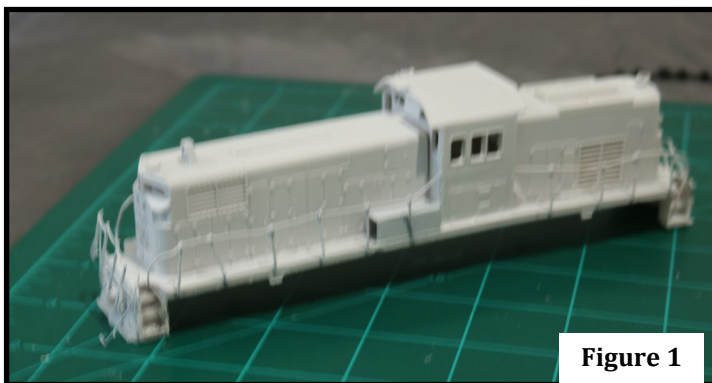


Figure 1

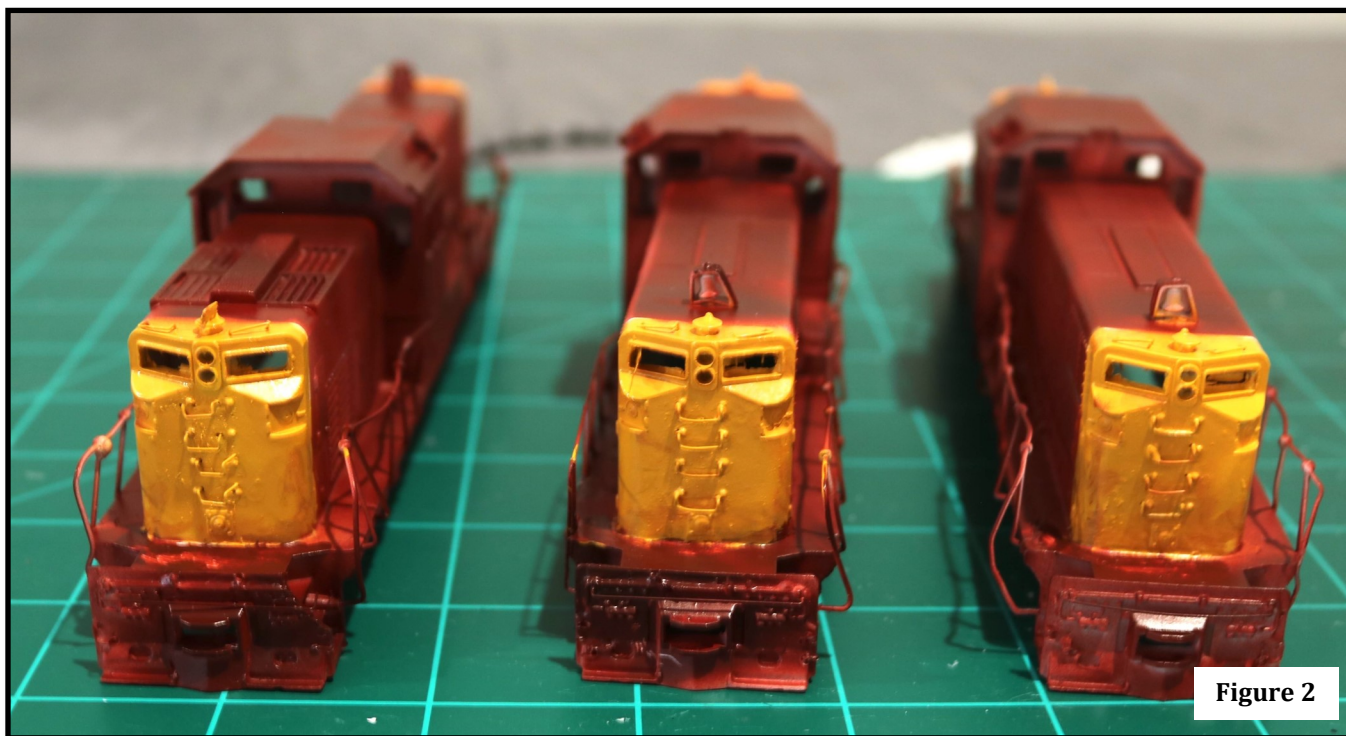


Figure 2

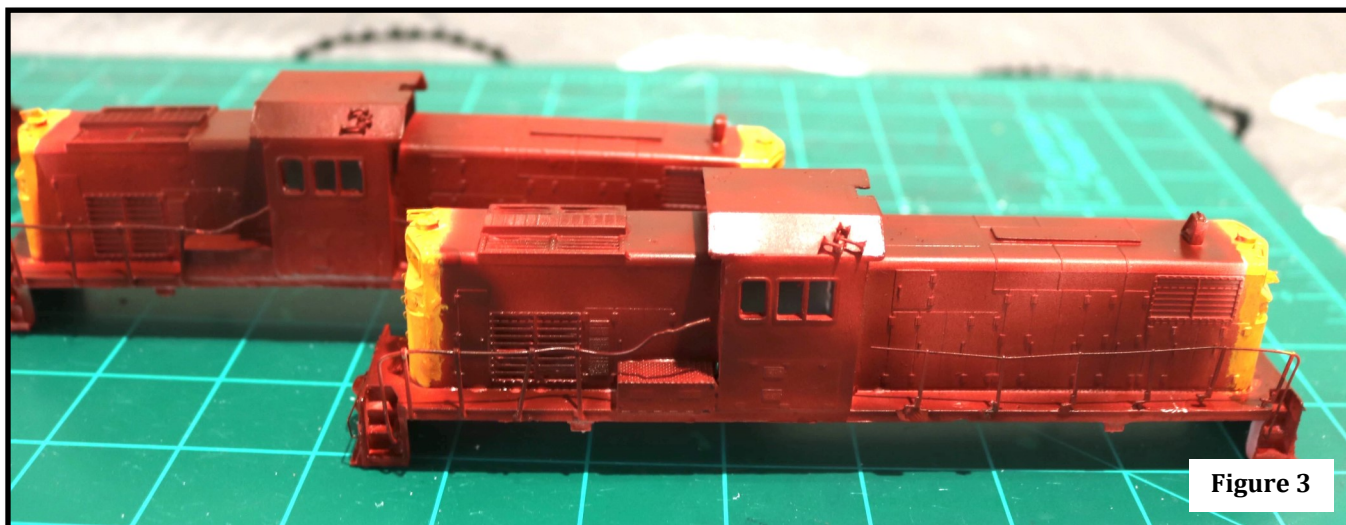


Figure 3

That Darn Bridge!

By Christine Heinsohn

In the June 2021 Brass Pounder starting on Page 9, David Heinsohn shared information on how traffic on the US waterways interacts with railroads at bridges. One of the bridges pictured on Page 10 of that issue is the Elizabeth River Lift Bridge in Norfolk VA..

http://www.mcor-nmra.org/Divisions/Kansas_Central_Division/BrassPounder/2021-06_V30-3.pdf

Norfolk Southern is the corporation responsible for the maintenance of that bridge. It is officially named Norfolk Southern #7 Railroad Bridge. It spans the Atlantic Intracoastal Waterway at statute mile 5.8 with a height above water of 7 feet when closed. The bascule bridge is used by both cargo and passenger trains and closes across the AICW on an as-required basis by a Norfolk Southern bridge tender who operates the closures and openings via remote control and cameras. It is adjacent to the Gilmerton Bridge which has a vertical clearance of 35 feet when closed.

Recently, this bridge has been causing a stir in the cruising community. The stirrings got me thinking that maybe modelers could take advantage of this “controversy” to enhance their layouts.

So first, what was the controversy? Norfolk Southern determined earlier this year that the #7 RR Bridge needed substantial maintenance. So they proposed the following to the US Coast Guard which is the controlling agency for closures of waterways in the US. (What follows has a bit of legalese to it!)

Proposed closure dates/times are provided below:

To begin the proposed maintenance, the bridge will be maintained in the closed-to-navigation position during (4) 64-hour closures from 12 a.m. to 4 p.m. on the following proposed dates in October:

- *Monday, October 4th through Wednesday, October 6th.*
- *Monday, October 11th through Wednesday, October 13th.*
- *Monday, October 18th through Wednesday, October 20th.*
- *Monday, October 25th through Wednesday, October 27th.*

To continue the proposed maintenance, the bridge will be maintained in the closed-to-navigation position during (3) 64-hour closures lasting from 12 a.m. to 4 p.m. on each of the following proposed dates in November:

- *Monday, November 1st through Wednesday, November 3rd.*
- *Monday, November 8th through Wednesday, November 10th.*
- *Monday, November 13th through Wednesday, November 15th.*

To conclude the proposed maintenance, the bridge will be maintained in the closed-to-navigation position during (2) 18-hour closures from 12 a.m. to 6 p.m. on the following proposed dates in November:

- *Thursday, November 18, 2021*
- *Monday, November 29, 2021*

At all other times, the bridge will operate as required by regulations. The vertical clearance of the bridge in the closed-to-navigation position is 7 feet above mean high water. Vessels able to pass through the bridge in the closed position may do so at any time.

The bridge will NOT be available during closure periods. Mariners should carefully consider transit plans outside of proposed closures and use caution when transiting the area. This information will be published in the Fifth Coast Guard District's Local Notice to Mariners.

For any questions or concerns regarding this issue, please contact the United States Coast Guard Sector Virginia Waterways Management Division duty officer at 757-374-3408 or by email at VirginiaWaterways@uscg.mil.

At first blush, all this seems like a reasonable request on the part of the Railroad. But boaters find it unreasonable. October and November are prime months for southbound traffic on the Atlantic Intracoastal Waterway. There will be boats stacking up in marinas to wait for the re-openings. The lateness of the proposed re-openings could cause safety issues because of the



shortened daylight hours of late fall. Boats arriving at marinas or anchorages delayed by the proposed 4PM bridge re-opening would be arriving after the sun had gone down, increasing the potential for mishaps. The boating community asked the Coast Guard to consider this safety issue. The Coast Guard amended the opening times – moving up the re-opening time from 4PM to 11 AM.

As a modeler, this situation could be an opportunity. Shown on this page are a couple of pictures, taken on October 22, 2015, when the bridge was raised for a scheduled re-opening during a multi-day closure of the bridge. These pictures give a sense of the volume of traffic on the water during the months of the proposed outage. For the modeler, this water traffic provides an opportunity for adding drama and even perhaps alternate movement to the layout.

Whether your bridge will actually open or not, you can create a story which will account for significant water traffic and maybe even maintenance of way workers and equipment!



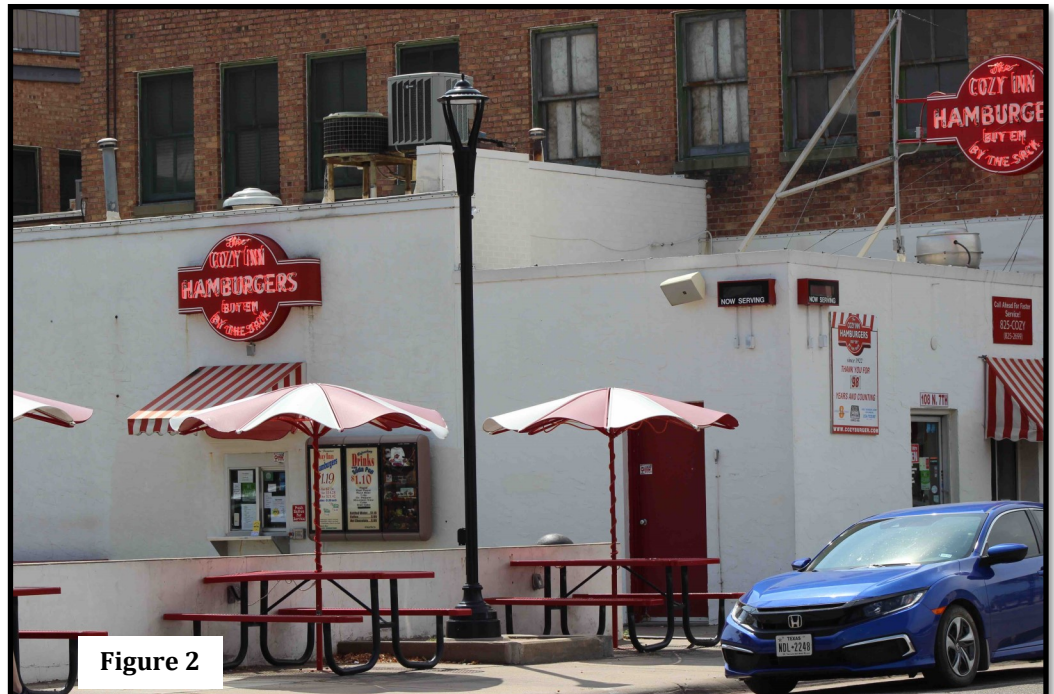
The Cozy Inn

By Tom Katsiafasz

The Cozy Inn in Salina, KS was first opened for business in 1922. The business itself has been the subject of many food reports on television as well as internet ratings. **Figures 1 and 2** depict the business as it currently exists.



I had laid out the walls and roof with .080 styrene. Since the building currently exists, I was able to get an accurate measurement and extrapolate the remainder measurements. I proceeded to cut out the openings for the doors, walkup window and the front window (**Figure 3**). One area of concern was the awnings and how those would be constructed. Along came City Classics with their recently released



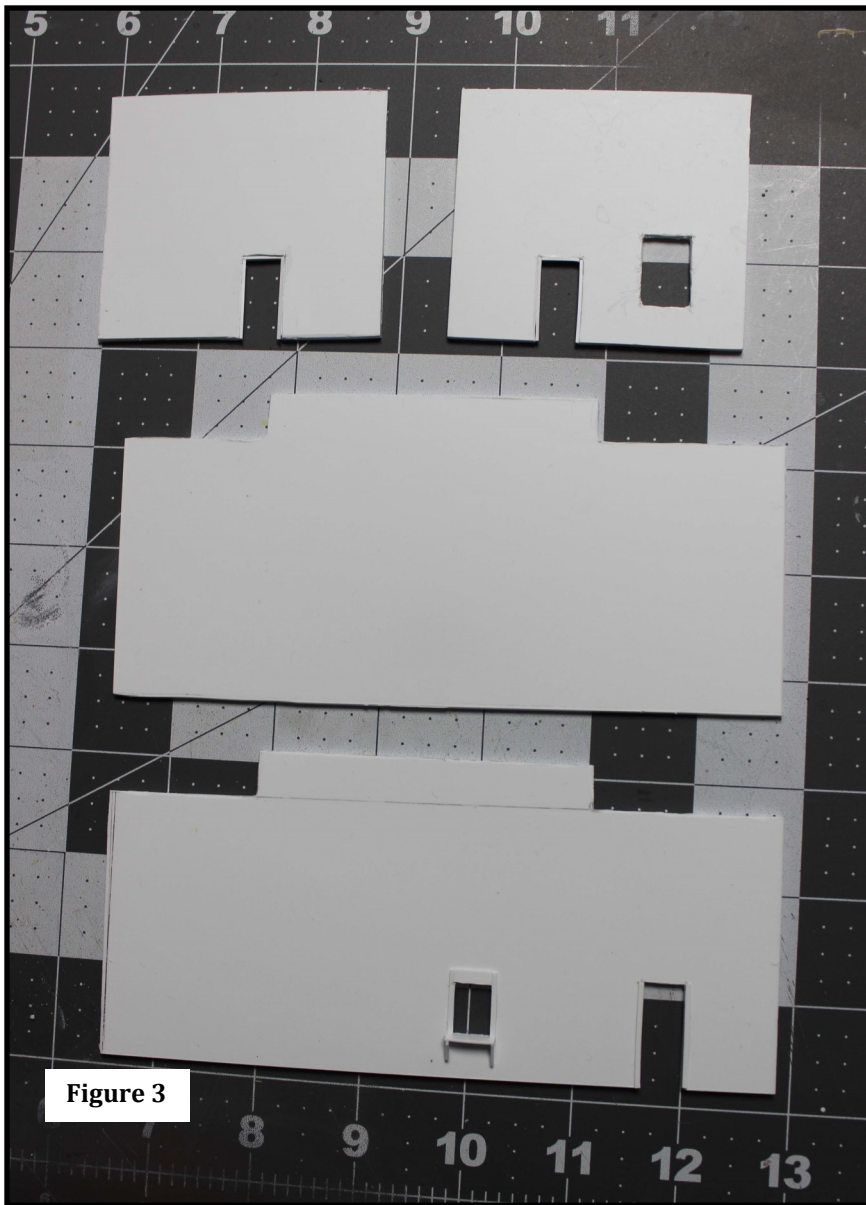


Figure 3

Speaking of signs, that posed another problem. The sign on the roof (see **Figure 2** upper right) on the front of the building was constructed of .019 wire, soldered together, and the sign attached with super glue. I was hesitant to attempt the soldering situation but it turned out suitable! **Figure 7** is a photo depicting the finished sign.

selection of different awning sizes. So that situation was solved. But how was I going to stripe the awnings? Initially I was going to hand paint stripes. BIG MISTAKE!! Then I thought that Microscale makes different width stripes, problem solved. (**Figure 4**).

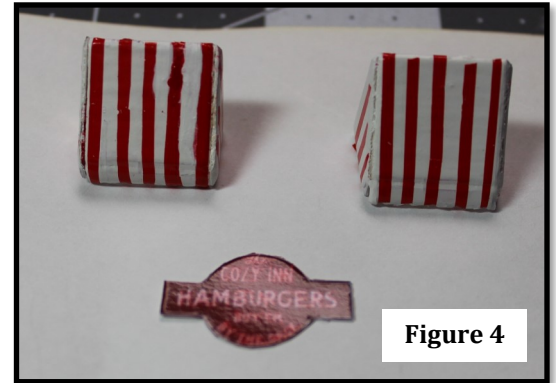


Figure 4

The actual walls on the building are a sort of plaster lath, not stucco, and can be seen in some of the photos. I applied lightweight spackling, being careful not to apply too much (**Figure 5**).

I had taken many photos of the business and, having obtained reasonable replicas of the signage, I used two (2) of these for the signs (**Figure 6**).



Figure 5

What is presented here is not the finished product as there are many details to be added later. There will be various vents on the roof and outside tables for outside dining to be added. The sign on the roof will also be added later.

That will be the subject of another article.

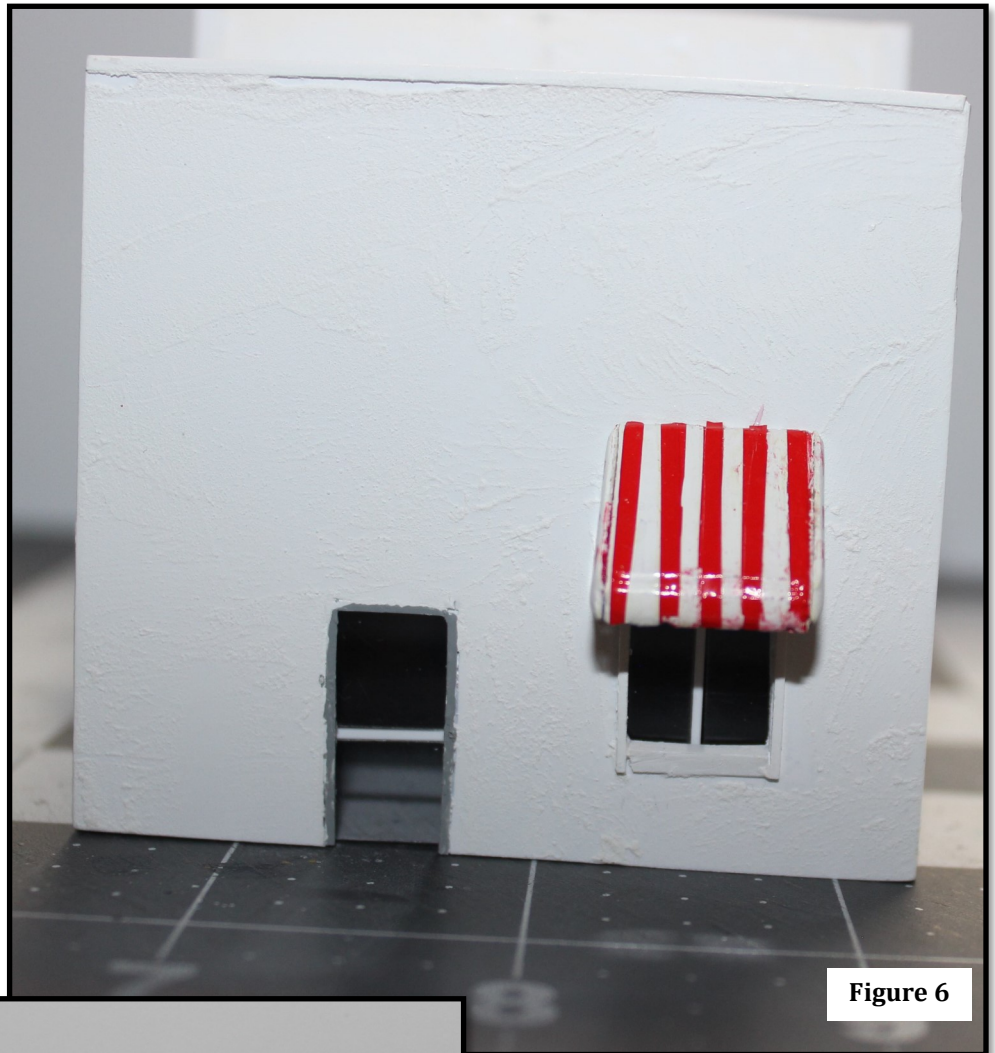


Figure 6

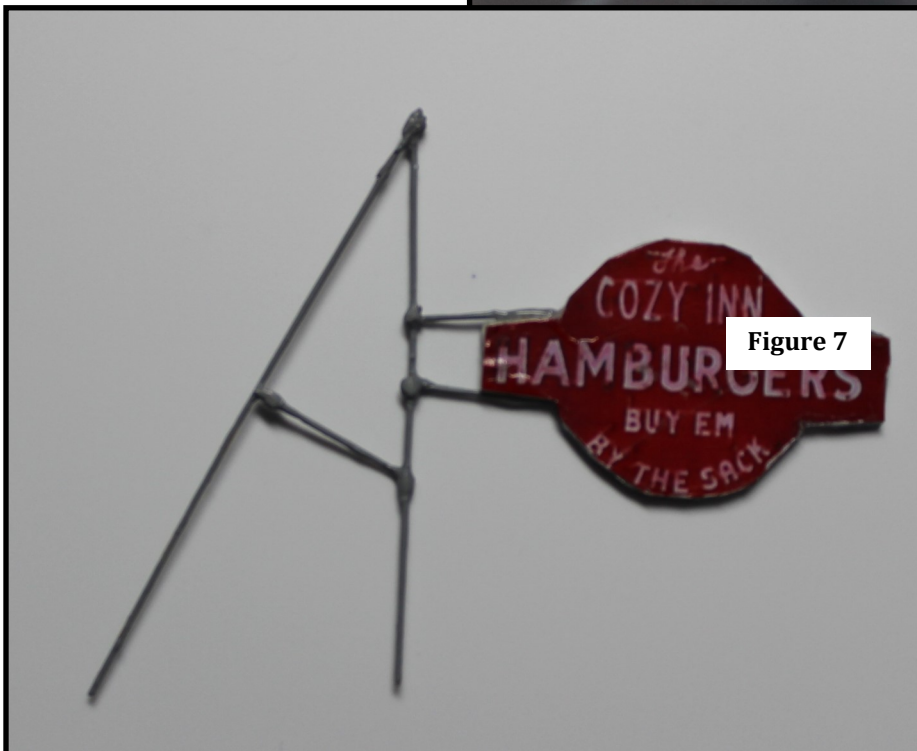


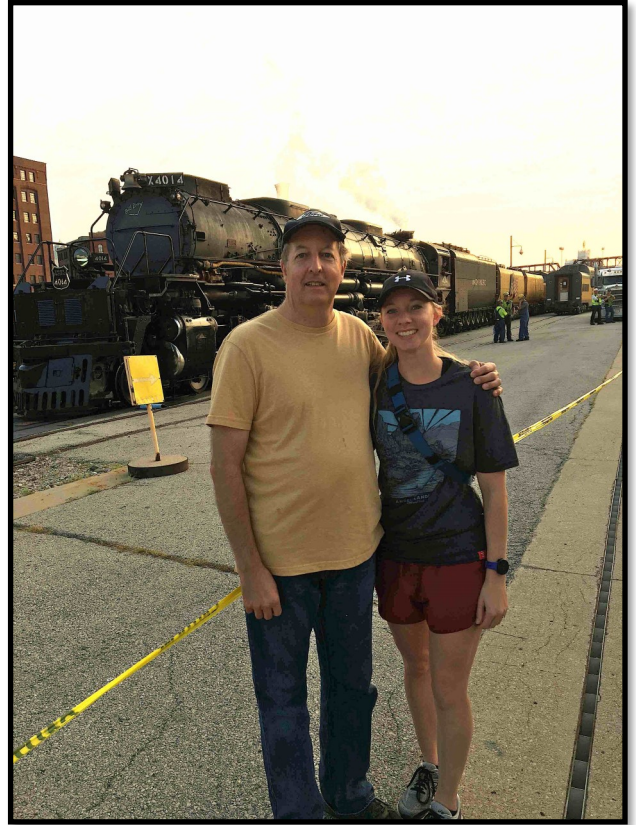
Figure 7

Chasing UP 4014

By Alan Meinholdt

With the passing of my dad in February 2021, I can say we enjoyed several trips following Union Pacific Steam around the state of Kansas as well as one trip that took us into Nebraska. On September 2, I continued that tradition with my youngest daughter Karen. We started our day at KC Union Station early in the morning (**Figure 1**) to watch 4014 leave, and our plan was to follow it all the way to Abilene. Our first stop was Bonner Springs to watch the train pass then on to Lawrence.

For some reason 4014 beat us to Lawrence. Plan B! So we waited on a county road just south of Midland KS. Luck was on our side and it paid off. We were able to follow the engine all the way around Midland and for a few short miles along the highway. The next place we stopped was just east of Topeka. Then we went on and waited just west of Silver Lake. Again, luck was on our side. We were able to be right beside the engine this time for several miles all the way through Rossville and Saint Marys. For a movie pacing 4014 at St Marys, go to this link :



https://www.dropbox.com/s/sx20argcwrt8xi2/IMG_2541%20copy.MOV?dl=0



About half way between Saint Marys and Bellevue we decided to get ahead of the train. By this time the train was slowing down to around 25 MPH. With the train going real slow through Bellevue, Karen came up with the idea of taking a picture with 4014 coming up in the background. For someone who wanted to turn around and watch, it was hard to stand there and look at the camera. All the time Karen was saying do not turn around yet. After it was taken and I looked at the picture and it was the shot of a lifetime (**Figure 2**).

By now we were getting hungry and we stopped in Manhattan for lunch and went on to an overpass by the Manhattan airport for what would be our last stop before heading home. By then it was around 4 in the afternoon and we decided not to go to Abilene. I had to take Karen back home to Gardner KS and, for me, another 80 miles to Emporia. It was a record day for a trip following steam. I drove 460 miles.

Minutes of the KCD Meeting

October 2, 2021

The ZOOM meeting was called to order by Superintendent Ross Boelling at 1:00PM. Members present were: Kelly Bruce, Ray Brady, Ross Boelling, Larry Tiffany, Alan Meinholdt, Pat Boltz, and Larry Shankles.

Minutes of the previous meeting on August 7, as contained in the October Brass Pounder, were approved as written.

Larry Tiffany reported on the KCD financial situation - no change.

With new member Kelly Bruce coming on board, the members went around the screen (remember - we were ZOOMing), introduced ourselves, and indicated our model railroad interests.

Ray pointed out that the joint Mid-Continent Region-Lone Star Region Convention

was coming up the following week (October 6-10) and all were encouraged to attend.

Ross talked a little about the Herington Days coming up on Apr 30-May 1 of next year but it may be premature whether the event will occur. With declining membership in the Herington Historical Society, our limited resources, and with breakthrough cases of COVID popping up, the event may be re-assessed - hence the KCD may not be a cosponsor of the event.

The remainder of the time was spent talking model railroading. Meeting adjourned.

*Respectfully submitted
Ray Brady, Acting Clerk*

Editor's Comments

By Ray Brady

Well, it is time to close out this issue of the Brass Pounder. This has been the 30th issue that I have edited spanning 5 years, and it is time to let someone else give a fresh approach to the news from the Kansas Central Division.

For me, this has been an incredible learning experience. I have gotten to know many of the KCD members and their modeling activities. I have learned from all of you because of your interests. None of us are alike in our modeling activities and that is exactly as it should be. We all are different and we learn from each other. Remember, it is your model railroading experience that counts, not how someone else thinks it should be.....

And, with that, I look forward to Greg Schneider continuing as editor of the

Brass Pounder. Welcome aboard Greg. And, remember to send him your articles for the Brass Pounder at:

gregoryschneider@sbcglobal.net

Now, I can hear three Union Pacific ALCO C-855 Double Diesel 3D-printed shells (two A-units and a B-unit set) beckoning me to come downstairs to bash them together with three ConCor GE U-50 chassis', as well as three decoders to build something unobtainable commercially in N-scale. You may hear about these (and more) in a future Brass Pounder....

*Ray Brady
Ex-editor of the Brass Pounder*



Timetable



-Mark Your Calendar-

Future Kansas Central Division [NMRA](#) Meetings

KCD Zoom Business Meeting

Saturday, December 4, 2021 @ 7:00PM @ Larry Tiffany's

KCD Elections for Superintendent and Director

Ballot emailed out by December 18, 2021

Voting deadline to return ballot to Larry Tiffany—January 10, 2022

KCD ZOOM Business Meeting

Saturday, February 5, 2022